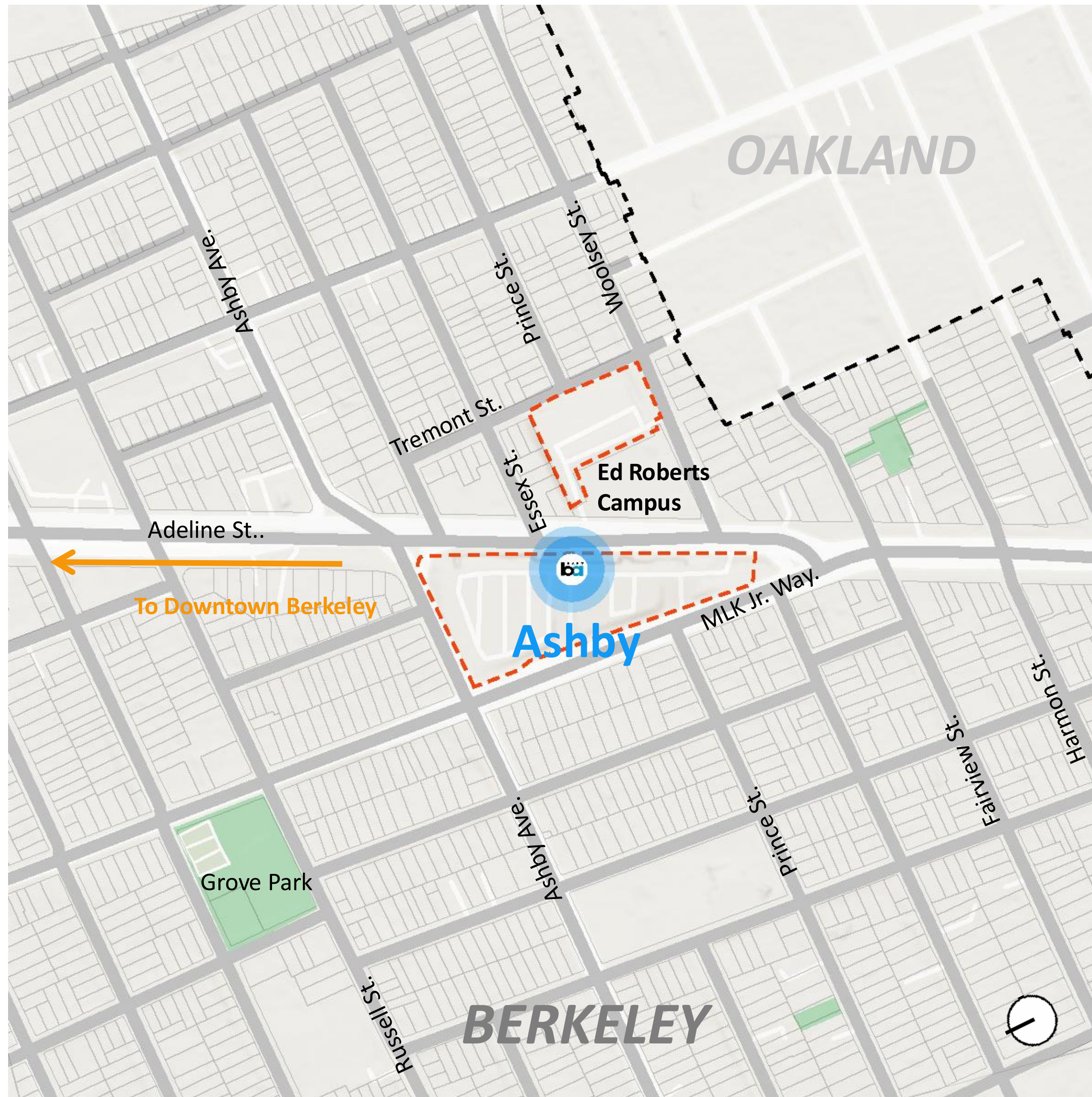


Welcome & Introduction

Overview

The City and BART have been closely collaborating to plan for much needed housing, a lively Adeline Street, and a new plaza that will be a permanent home for the Berkeley Flea Market and other public uses, while continuing to ensure access to the Ashby BART Station.

The purpose of this community open house is to provide updates and hear your thoughts and questions about key parameters and next steps that will shape future development.




Navigating Around



Topic

Station

 Welcome & Introduction	1
Roadmap for Future Development at Ashby BART	2
Getting To, From, & Around Ashby BART	3
Adeline Street Redesign & New Plaza	4
Activating Adeline Street	5
BART Traction Power Substation (TPSS)	6

Special Meeting of the Berkeley City Council

Monday, Sept. 16, 2024, at 6pm
School District Board Room –
1231 Addison St.

In person or Zoom link:

<https://berkeleyca.gov/your-government/city-council/city-council-agendas>



Welcome & Introduction

How We Got Here & Where We Are Going

Sep 2018

- Assembly Bill 2923 is passed. Since then, the City and BART, with active community participation, have been collaborating on advancing transit-oriented development (TOD) at the North Berkeley and Ashby BART station areas.



Mar 2020

- City and BART sign Memorandum of Understanding (MOU)

Apr 2021

- City Council reserved \$53M in City Affordable Housing funding for both station areas

Jun '20 – Jun'22

- The City held Community Advisory Group and community-wide meetings to develop zoning standards and vision and goals for both station areas

Jun 2022

- City Council adopted Residential-BART Mixed Use (R-BMU) Zoning
- City and BART approved Joint Vision and Priorities for Ashby and N. Berkeley (JVP)
- City and BART signed Memorandum of Agreement (MOA)

Nov 2022

- City Council approved a preferred concept to redesign of Adeline Street at Ashby BART

Jan '23 – Aug'24

- City and BART negotiations about the City's air rights over the Ashby BART West Lot and potential community benefits and other project requirements

Sep 2024

- City Council Meeting: Ashby BART Term Sheet

Nov 2024

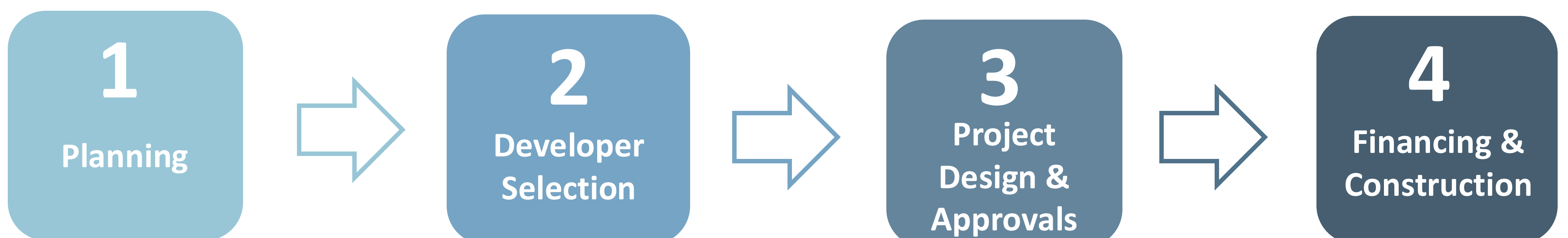
- City Council Meeting: Ashby BART Real Estate and Other Legal Agreements

Dec 2024

- Issuance of Developer Solicitation for the Ashby BART West Lot

Phases of Development

We Are Here



Roadmap for Future Development

Overview

City and BART staff negotiated a “term sheet” to serve as a roadmap for future development. The draft term sheet lays out proposed agreements about property ownership, community benefits, and roles and responsibilities for future development for the City, BART, and future developer(s).

If approved by the City Council at their September 16th meeting, the proposed agreements will be turned into binding legal documents that will be brought back to Council for final approval later this fall, which will open the door for starting the developer selection process.

Key Elements of Proposed City and BART Term Sheet

Land-for-Air-Rights Swap

The City holds an option to purchase the “air rights” (starting at 10 feet above the ground) to the West Lot. As proposed, the City will give up its “air rights” to the Ashby BART West Lot in return for BART giving the City sole ownership of the East Lot, and specified community benefits and other requirements for the West Lot and East Lot, respectively. Simplifying ownership of the parcels lessens the uncertainty for future developers.



Roadmap for Future Development

Key Elements of Proposed City and BART Term Sheet (continued)

Affordable Housing

- At least 50% of the first 602 new housing units developed on the West Lot (or 301 units) must be affordable to households with extremely low, very low, low and moderate incomes.
- If the future development on the West Lot exceeds 602 units, at least 35% of the total number of housing units must be affordable, as specified by the term sheet (including any additional units, whether density bonus or otherwise, beyond the initial 602 units).
- Development on the East Lot must include at least 35% affordable units (as specified by the term sheet).
- The City will make available \$26.5 million of its affordable housing funding, of which \$18.5 million is reserved for the West Lot and \$8 million is reserved for the East Lot (which is roughly proportional to the parcel sizes and development capacity).

Who is served by affordable housing?

Scan to find out!



Berkeley Flea Market

- A permanent home for the Berkeley Flea Market.
- The redesign of the Adeline Street right-of-way will result in a "road diet" and new plaza that will support the Berkeley Flea Market and other public programming.
- Future development on the West Lot will include at least 5,000 sq. ft. ground floor space in the building for community/civic commercial uses (e.g., for Berkeley Flea Market) at no more than 50% of market rate rent.
- See **Station 4** for more details on the planned redesign of Adeline Street and Station 5 about "Activating Adeline Street."



Maudelle Miller Shirek Affordable Housing



The Berkeley Flea Market

Community Benefits Fund

- The City will require that a portion of the revenues from the East Lot development parcels shall be allocated to create a reparative community benefits fund for South Berkeley.

Roadmap for Future Development

Key Elements of Proposed City and BART Term Sheet (continued)

Connections to Adeline Street

- See [Station 5](#) for detailed information

Traction Power Substation (TPSS)

- See [Station 6](#) for detailed information

Public Infrastructure

- Future transit-oriented development at Ashby BART will require major investment in public infrastructure.
- The term sheet identifies 24 types of public infrastructure, potential funding sources, priorities, and responsibilities in the event of funding shortfall and for maintenance.
- Potential funding sources include - state, federal and local grants, bond funds, special funding districts (e.g. Enhanced Infrastructure Financing Districts and Community Financing Districts), and developer contributions.



Developer Selection

- The City and BART will each pursue separate solicitations for development for the West Lot and East Lot, respectively.
- A "short list" of developers will make a public presentation as part of the selection process.
- For the West Lot: The selection committee will be comprised of 7 members: (4) BART representatives, (2) City representatives and (1) representative from Equitable Black Berkeley.
- For the East Lot, the City will collaborate with BART to provide input and guidance on the proposals received. However, BART will not participate in the solicitation process other than to ensure that BART's access and operations requirements are satisfied and that the East Lot includes a minimum of 300 bedrooms.

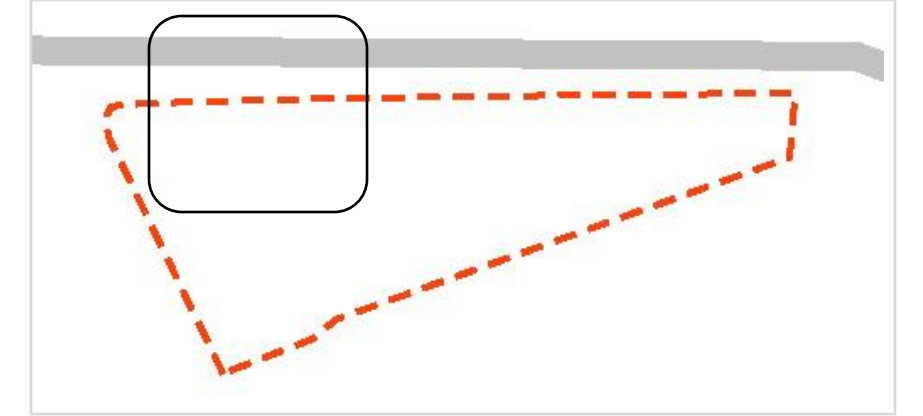
Objective Design Standards

- The term sheet outlines the process to develop Objective Design Standards (ODS) for the East Lot and West Lot.
- The Berkeley community, BART and the selected developer(s) will be given the opportunity to provide input into the ODS to ensure that the resulting document is consistent with what can feasibly be developed and aligned with the intent of the City and BART Joint Vision and Priorities document.
- The term sheet defines the development capacity that the ODS must allow for the West Lot.
- For the East Lot, the ODS must allow a development capacity of at least 300 bedrooms.

Overview

Development on the West Lot will dramatically transform the way BART riders and community members navigate the area by limiting car access and creating walking and biking paths throughout the site. Two important requirements inform where development can occur on the site:

- Easy access for pedestrians and bikes from Adeline Street and MLK Jr. Way to the Main Station Entrance
- Emergency responder access to the station.



Transportation changes in and around the station area



- | | | |
|---|---|--|
| <ul style="list-style-type: none"> ① BART Station Entrance ② Elevator Access ③ Secure Bike Parking ④ Vehicle Parking Access | <ul style="list-style-type: none"> → Vehicle Access ↔ Pedestrian Access ⋯ Pedestrian Access through TOD ⌋ Plaza Stair ⓧ North Stair and ADA Ramp (to be removed) | <ul style="list-style-type: none"> ↻ Bicycle Path of Travel ⋯ Shared Loading Zones ⋯ Bus Loading Zone ■ Development Area A, B, and C |
|---|---|--|

Station Access

- BART rider drop-off/pick-up areas
 - Adeline Street (main)
 - MLK Jr. Way (secondary)
- Walk, bike, wheelchair, and other rolling devices from:
 - MLK Jr. Way and Adeline Street near main station entrance
 - MLK Jr. Way through the future TOD
- New and upgraded bikeways on Adeline Street and MLK Jr. Way
- Adeline/Ashby corner access removed, converted to plaza
- Vehicle access to parking between Development Areas A & B

How emergency routes will shape the site's design



- | | | |
|---|--|---|
| <ul style="list-style-type: none"> ▶ BART Station Primary Entrance ▽ BART Station Emergency Egress ■ Point of Safety and Safe Dispersal Area → Emergency Vehicle Access (EVA) ① Primary Access Point to Main Station Entrance (clear path of travel to MLK Jr Way) | <ul style="list-style-type: none"> ② Station Entrance Plaza EVA/Maintenance Turn-around ③ Primary Access Point to TPSS/North Platform Emergency Egress Stair (Clear path of travel to MLK Jr Way) ④ TPSS EVA/Maintenance Turn-around ⑤ Secondary Access to Station Entrance Plaza ⑥ North Plaza Stair (to be rebuilt) | <ul style="list-style-type: none"> ⑦ North Platform Egress Stair ⑧ Main Station Egress ⑨ South Platform Egress Stair Exit ⑩ Ed Roberts Station Egress Stair ⑪ Elevator Access ⑫ Adeline Main Plaza Emergency Vehicle Access |
|---|--|---|

Emergency Vehicle Requirements

- The primary emergency access point to the station is the Station Entrance Plaza
- Berkeley Fire Department requires two points of access to the Station Entrance Plaza; this will require secondary emergency vehicle access through Development Area B, (location is flexible).
- Emergency vehicle access to the TPSS requires a drive between Development Area A and B, the location is flexible.

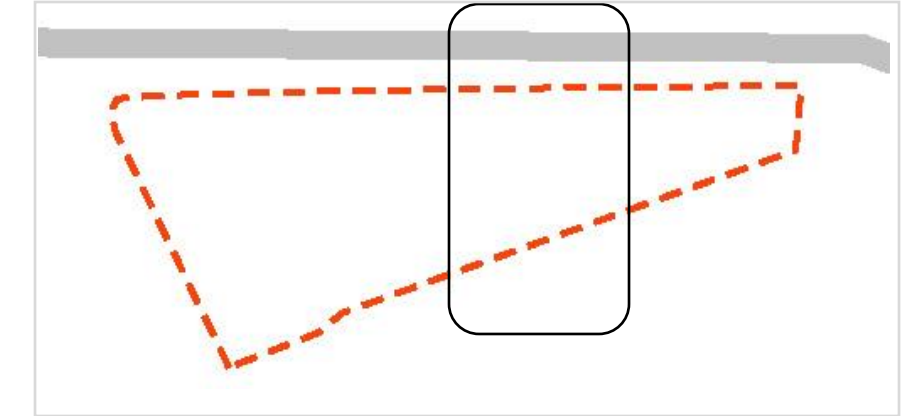
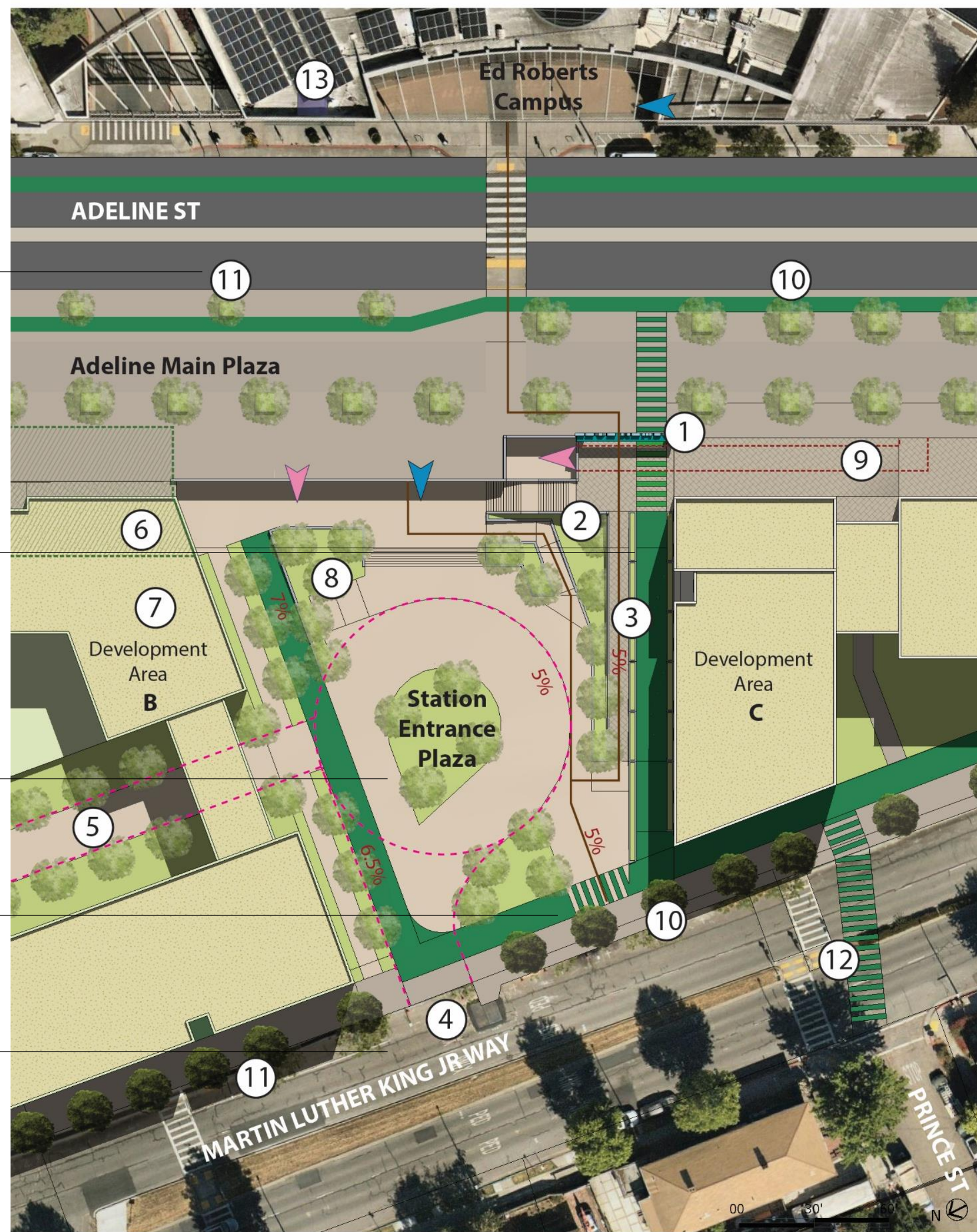
Getting To, From, & Around Ashby BART

Station Entrance Plaza

A new Station Entrance Plaza creates a new public open space and way to get to the station. The station entrance is about 15 feet below Adeline Street and 10 feet below MLK Jr Way. To improve access to all BART riders, a new pathway/ramp will connect from Adeline Street to the Main Station Entrance.

Key Features

- Loading area on Adeline Street
- New Pedestrian /Bike Ramp and Stair connecting to Station Entrance
- Pedestrian Plaza (No Private Vehicles)
- Cycletrack on MLK Jr. Way
- Loading area on MLK Jr. Way

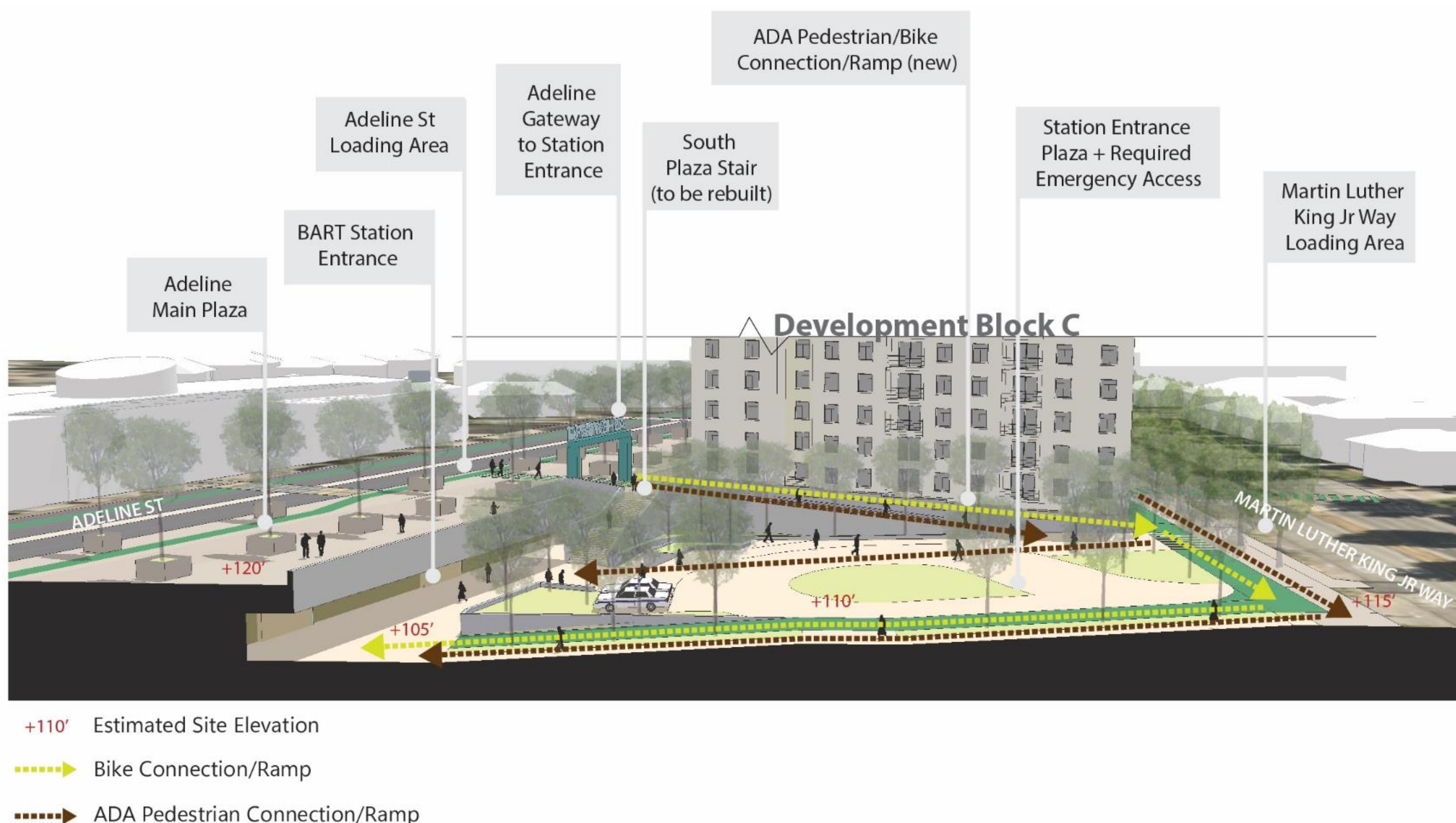


- BART Station Entrance
- BART Station Emergency Egress
- 5% Estimated Slope
- ADA and Pedestrian Access
- 1 Station Gateway
- 2 South Plaza Stair (to be rebuilt)
- 3 Pedestrian/Bike Access Routes (<5% grade)
- 4 Primary EVA/Maintenance Access
- 5 Secondary EVA Access
- 6 Secure Bike Parking
- 7 BART Rider and Staff Parking
- 8 Police Parking
- 9 Tunnel to South Platform Emergency Egress Stair
- 10 BART Rider and Residential Loading Areas
- 11 Bus Stop
- 12 Prince St Enhanced Ped/Bike Crossing
- 13 Elevator Access

Sample design concept only. No design for this project exists at this time. Designs will be prepared after developers are selected. The graphic shows possible areas for buildings and required areas for station access, operations, and emergency response routes.

Getting to and from the main station entrance in the future

Sample design concept to illustrate key circulation features and grade changes



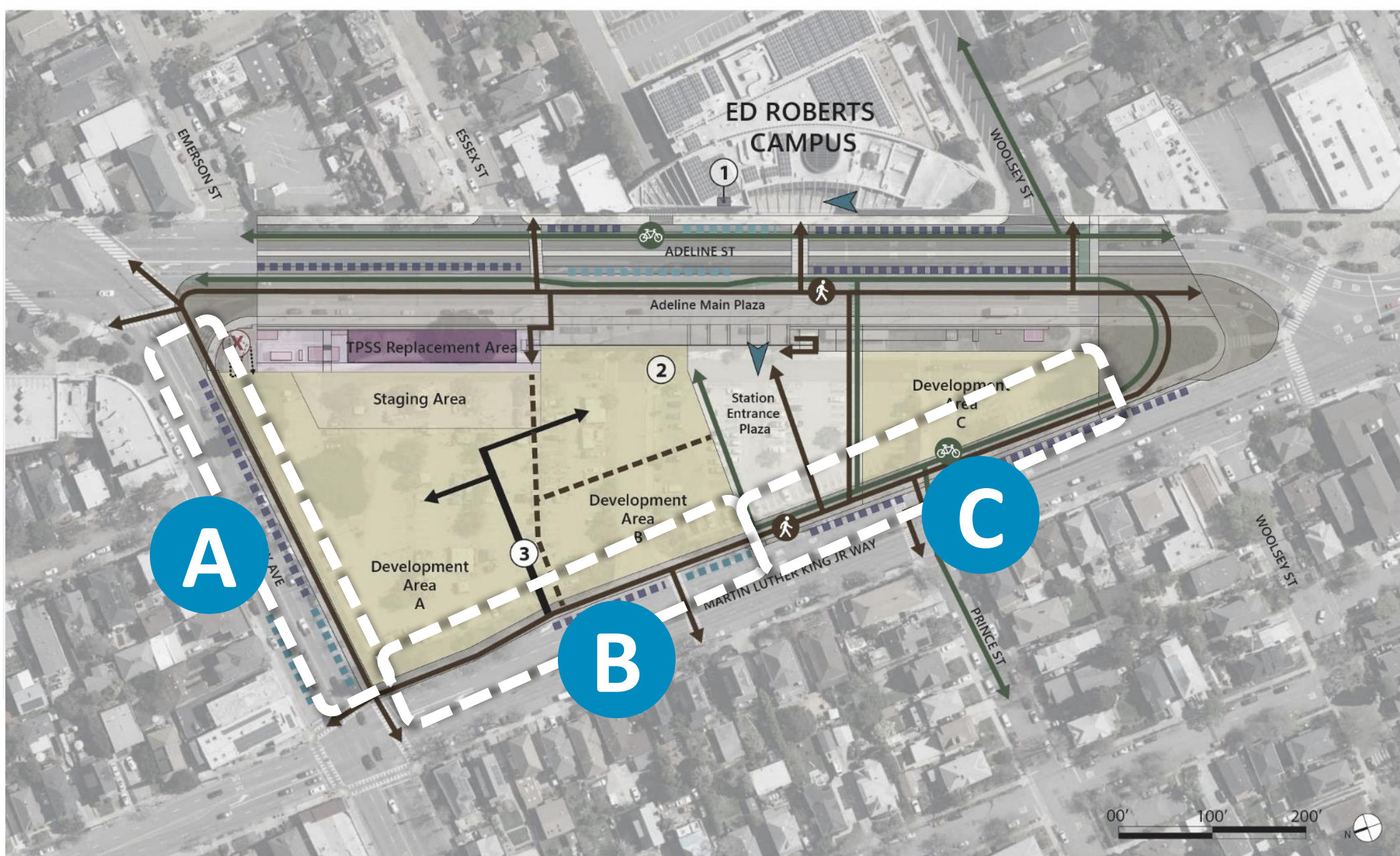
- +110' Estimated Site Elevation
- Bike Connection/Ramp
- ADA Pedestrian Connection/Ramp

Improvements to surrounding streets

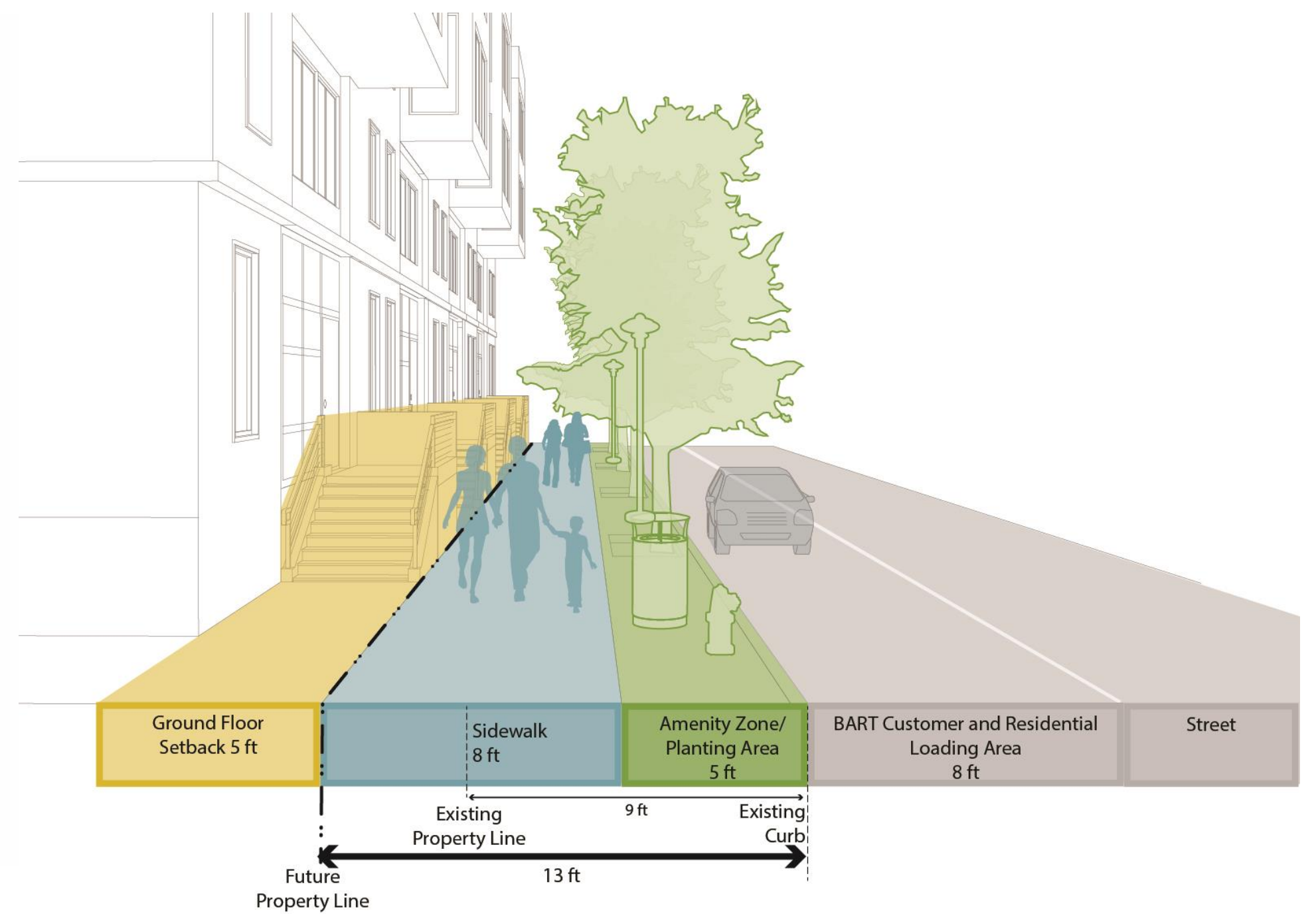
The West Lot developer will be required to upgrade MLK Jr. Way and Ashby Avenue to standards identified in the *Adeline Corridor Specific Plan* for segments next to the West Lot, such as widened sidewalks and bikeways. These changes will be coordinated with work that the City is leading on MLK Jr. Way and Woolsey Street to improve bus stops and roadway crossings. Adeline Street is detailed in **Station 4**.

The diagrams below illustrate concepts to improve the pedestrian environment on Ashby Avenue and MLK Jr. Way to respond to the future adjacent development.

Key Map: Locations of street section diagrams



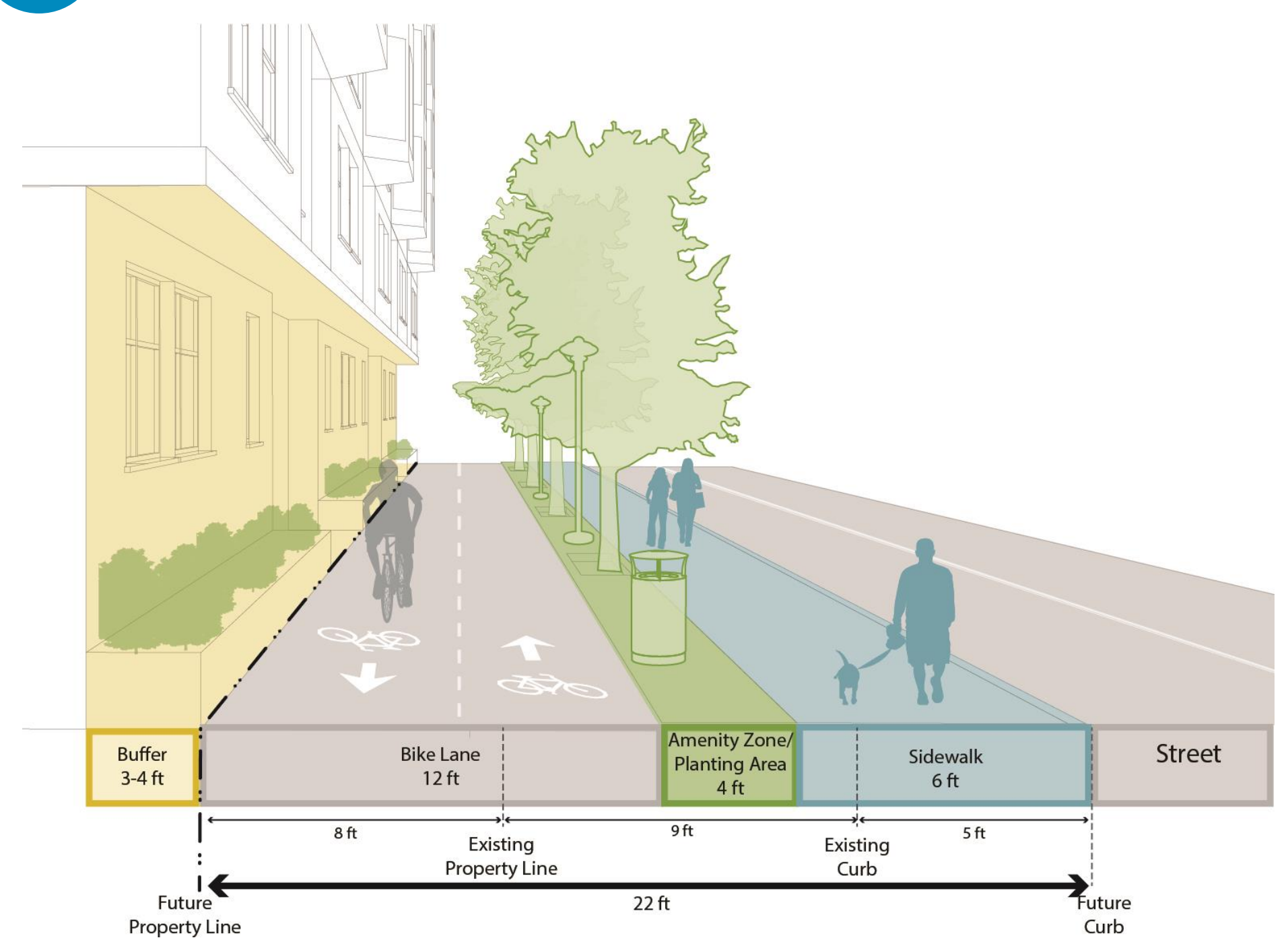
A Ashby Avenue Street Section Diagram



B MLK Jr Way Street Section Diagram (North)



C MLK Jr Way Street Section Diagram (South)



Changes coming in advance of TOD

BART and the city of Berkeley are working on near-term construction projects to improve walking, biking, and bus transit in and around the station area. These projects will help set the stage for improvements led by the future TOD developer. Scan the QR codes below to learn more:

Berkeley's Shattuck-MLK Bus Stops Project



Berkeley's Woolsey-Fulton Bike Boulevard Project



BART's Ashby Bicycle Access Improvements



See handout

Adeline Street Redesign & New Plaza

Overview

This City of Berkeley project will redesign Adeline Street to make it safer for those walking, biking, rolling, or using transit, and create new opportunities for public space. In front of the Ashby BART Station, this redesign will reduce Adeline from 4 to 2 lanes, repurposing the two lanes closest to the BART station as a public plaza that can be used for the Berkeley Flea Market, Juneteenth, and other events. The project is a result of concepts developed during in the Adeline Corridor Specific Plan.

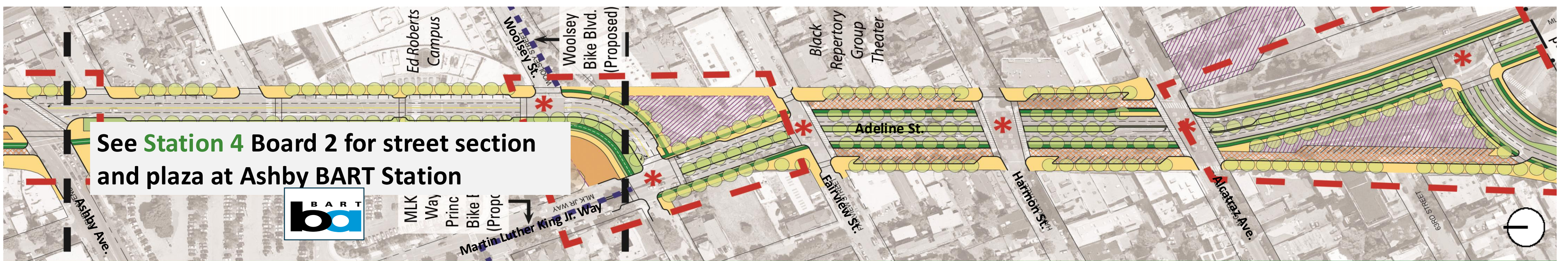
Purpose & Goals

- Improve safety for all modes of transportation
- Close a gap in the bicycle route between Downtown Berkeley and Downtown Oakland
- Create new public space
- Improve transit operations
- Support local businesses and affordable housing

Proposed Project Elements

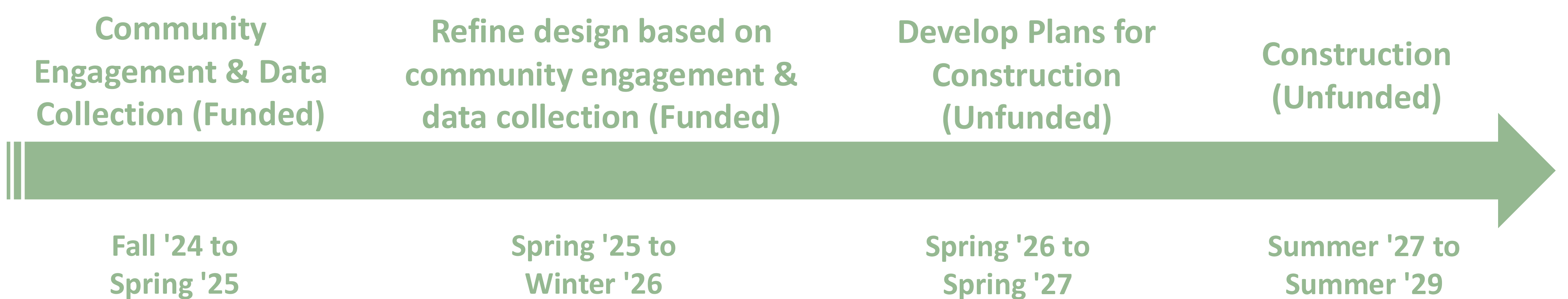
- Reduce number of traffic lanes
- Improve pedestrian visibility and shorten crossing distances
- Physically separate & protect bike lanes
- Upgrade bus stops with bus bulbouts, shelters, and benches
- New public plaza space at Ashby BART TOD
- Loading zones for businesses, passengers, and Paratransit

Adeline Street Design Concept from the 2020 Adeline Corridor Specific Plan



- Public Space Opportunity Area: may include landscaped areas, plazas and programmed events
- Sidewalk
- Landscaped medians and buffers
- 2-Way Cycle Track (Class 4 Bikeway) (Occurs on West side between Russell Street and MLK Jr. Way)
- 1-Way Cycle Track (Class 4 Bikeway)
- Proposed Bike Blvd
- Existing Bike Blvd
- Low-speed drive aisles for access to properties, parking, and drop off zones, as well as for Fire Dept vehicles
- Trees (denoting rows of trees rather than individual locations)
- Detailed design of pedestrian and bicycle treatments at intersection not shown, will occur during later design phases.
- Area subject to further refinement and/or assessment of options during future planning and design phases, including street alignment, intersection geometry, public space design and programming, or configuration of opportunity area for community-oriented facility or affordable housing.
- Opportunity Area for potential public space and/or development (additional study required)

Estimated Project Timeline

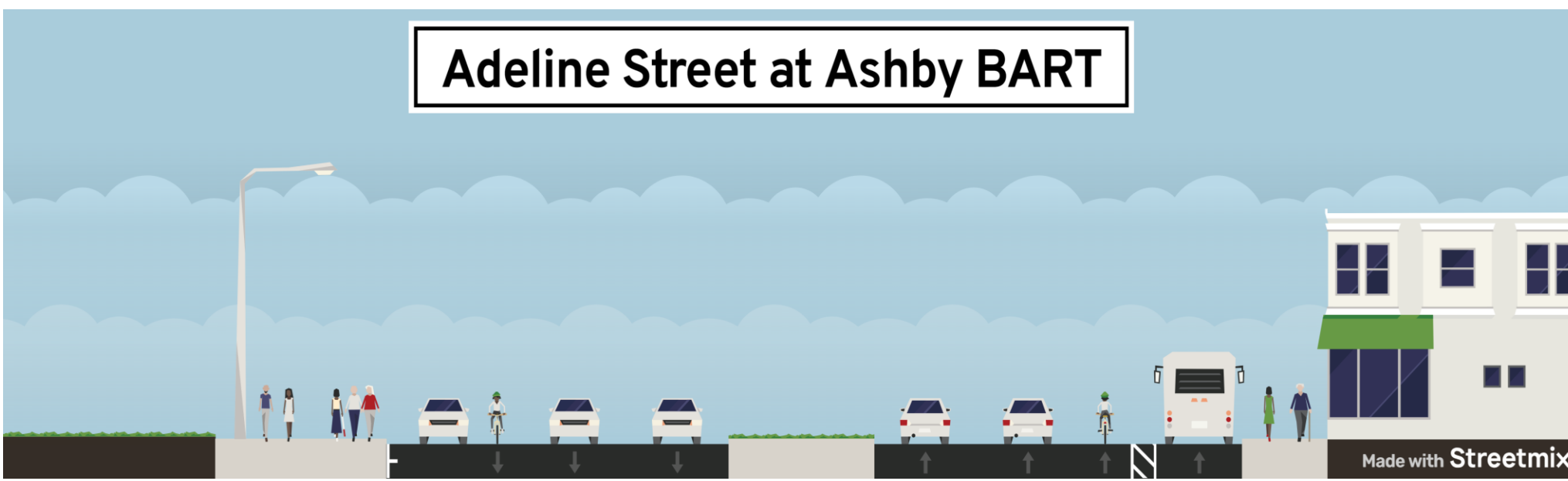


Adeline Street Redesign & New Plaza

Overview: Adeline Street - Plaza

By repurposing two of the existing four lanes of Adeline Street, a new public plaza can be created to serve as community open space, a location for the Berkeley Flea Market, and community events like Juneteenth.

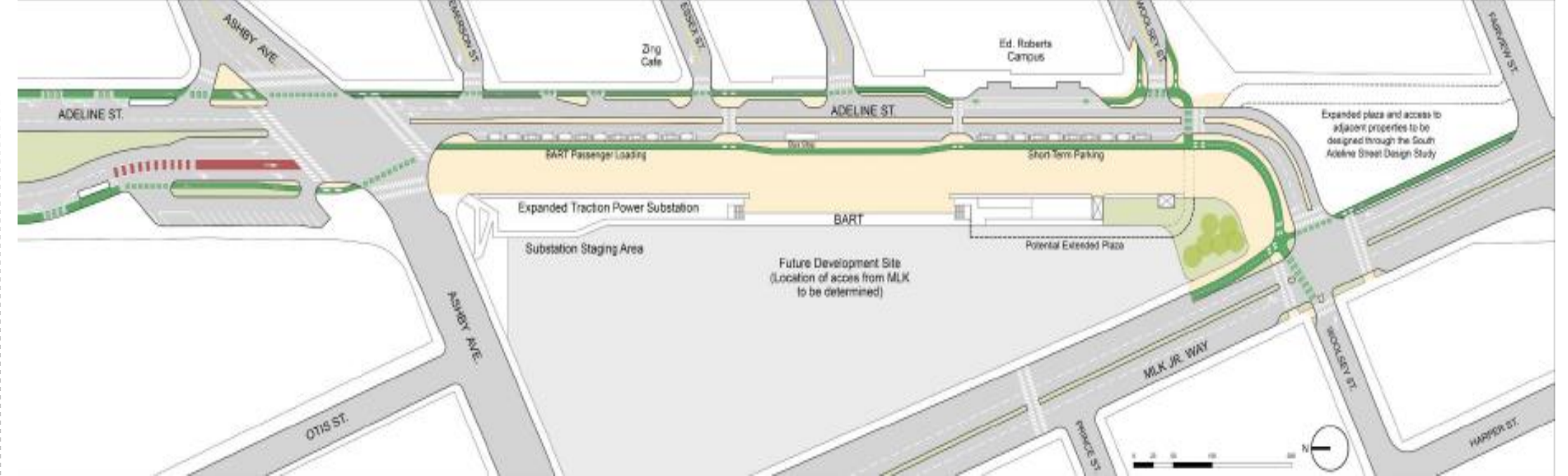
Existing



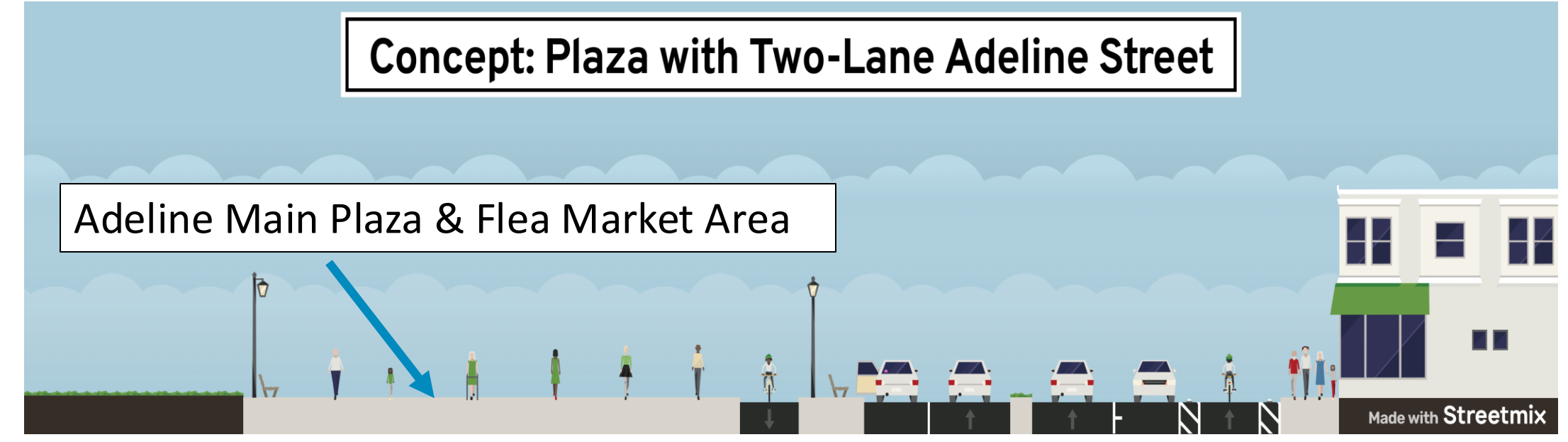
Key Points:

- Two vehicle lanes in each direction
- Bus stops along Adeline (Line F)
- Bike lane on both sides of Adeline, but southbound cyclists must share lane with cars at MLK Jr. Way
- Berkeley Flea Market uses BART Parking Lot

Proposed



Concept approved by Berkeley City Council on November 29, 2022



Key Points:

- New public plaza for Berkeley Flea Market and events
- One traffic lane in each direction
- Improved bus stops
- Passenger pick up and drop off for Ashby BART Station
- Curbside loading zones for Flea Market merchants
- Physically Separated/Protected bicycle lanes
- Better pedestrian visibility and shorter crossing distances
- Upgraded traffic signals
- Upgraded lighting

Final design will be determined through public engagement, traffic and transit operations analysis, and review of design criteria.

Plaza Case Study: Downtown Berkeley BART Station

BART and the City of Berkeley worked together to design and deliver the Downtown Berkeley BART Station, which opened to the public in October 2018. The space serves as an access point to the BART station, outdoor seating, features public art, and events. The plaza is maintained and programmed by the Downtown Berkeley Association under contract with the City.



Images: Bartable, Downtown Berkeley Association

Activating Adeline Street

Overview

Activating Adeline Street frontage is a high priority (e.g. City and BART draft term sheet, Joint Vision and Priorities for Ashby BART and North Berkeley BART and the Adeline Corridor Specific Plan). This can be accomplished through building future buildings on the West Lot that connect to Adeline Street and by promoting uses on the ground floor of the buildings and in the new future plaza area that encourage more residents and visitors to come for shopping, dining, and arts and cultural experiences.

Note: No design for this project exists at this time. This graphic shows one possible layout of buildings and required areas for station access, operations, and emergency response routes.

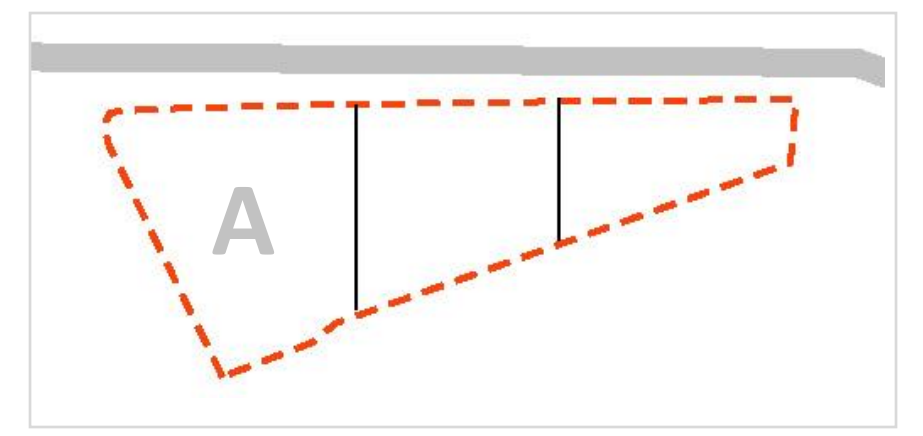


- There are 3 areas where future buildings can potentially be built to connect to Adeline Street (Areas A, B and C).
- Other areas have constraints due to BART station safety, access and operational requirements, e.g. the Traction Power Substation (TPSS) and BART station entrance.
- Future developer(s) will need to further study the design and cost of constructing buildings adjacent to the BART tunnel and other infrastructure.
- The draft term sheet identifies connection at Area C as a project requirement; connections at Area A and at Area B as a high priority if feasible.
- While extensive additional analysis and design will need to be done by future developer(s), preliminary design and engineering analysis have shed light on opportunities and issues for the future development to connect to Adeline Street for each of the three areas.

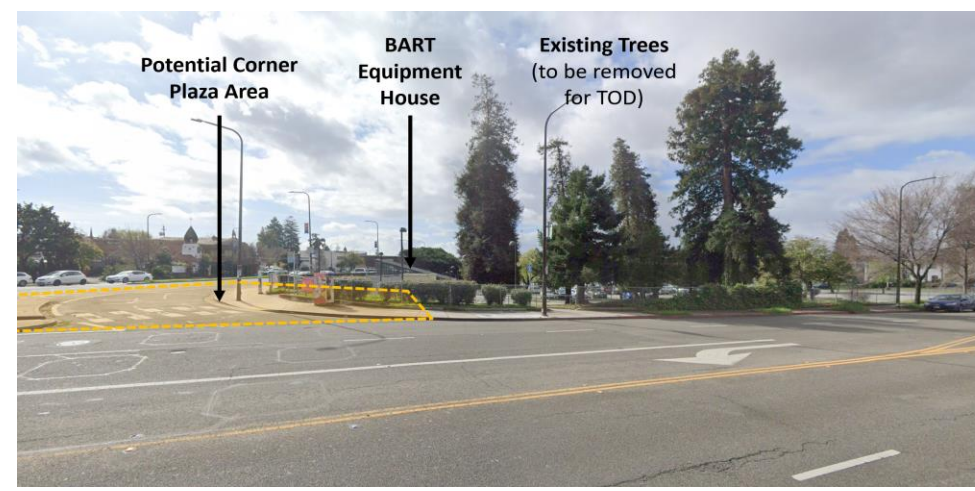
Activating Adeline Street

Area A: Corner of Adeline & Ashby

The corner of Ashby & Adeline Street is a visible and important intersection and a gateway to future development. Relocating the stair and ADA ramp to the parking lot, development can potentially be located up to a new corner plaza, mirroring the existing building across Ashby Avenue. **Sample design concept only. No design for this project exists at this time.**



- +110' Estimated Site Elevation
- Estimated BART Property Line
- ① Existing Building Frontage
- ② Potential Building Frontage on Corner Plaza
- ③ Corner Plaza
- ④ Corner Plaza Extension
- ⑤ TPSS Equipment + BART Vents



From Ashby/Adeline Corner



From Parking Lot

Opportunities

- “Anchor” the corner with a new building that is aligned with the building location across Ashby Avenue to the north
- Create new public space at the corner
- Integrate corner plaza with ground floor commercial space
- Provide opportunity for outdoor seating

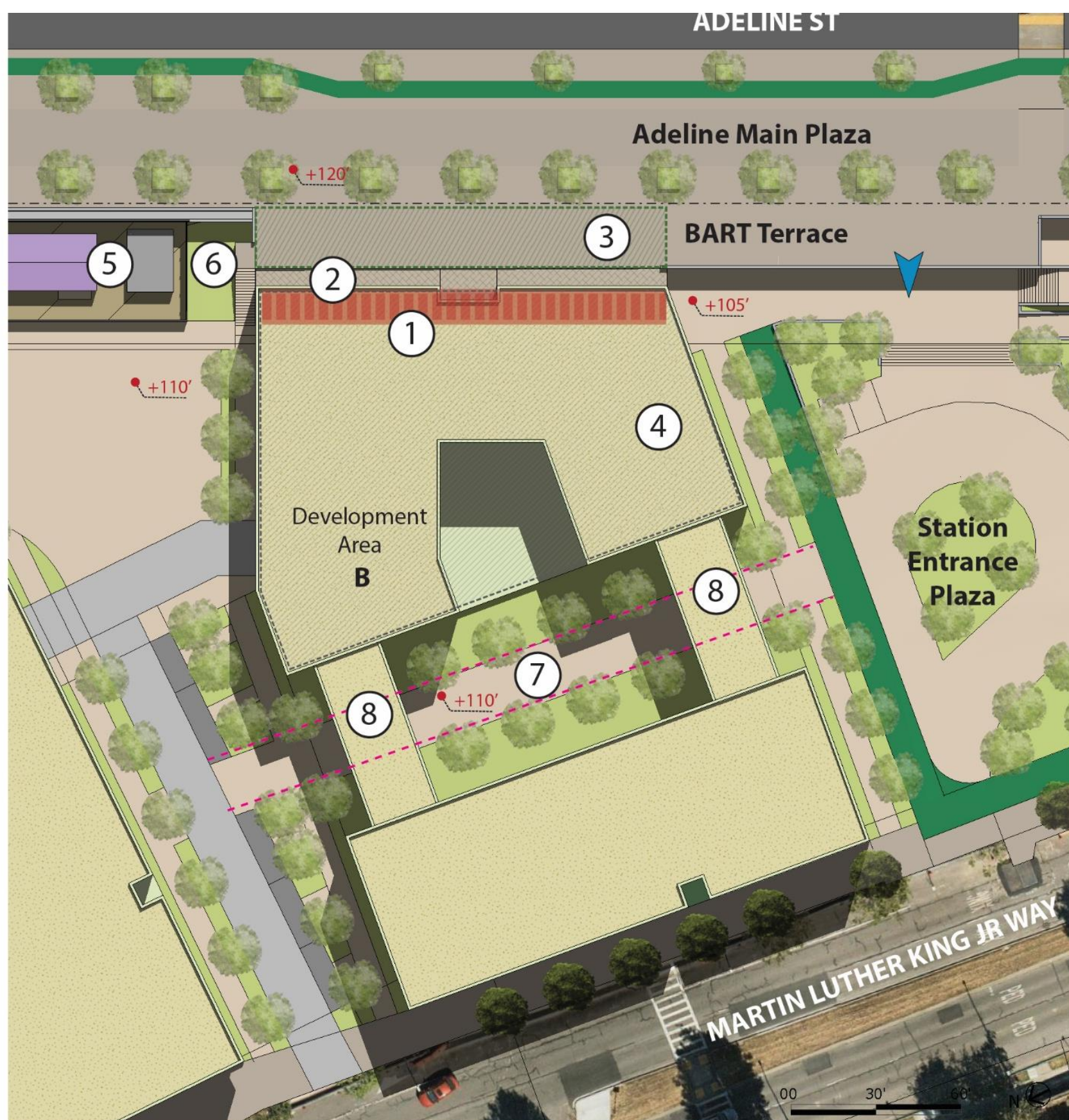
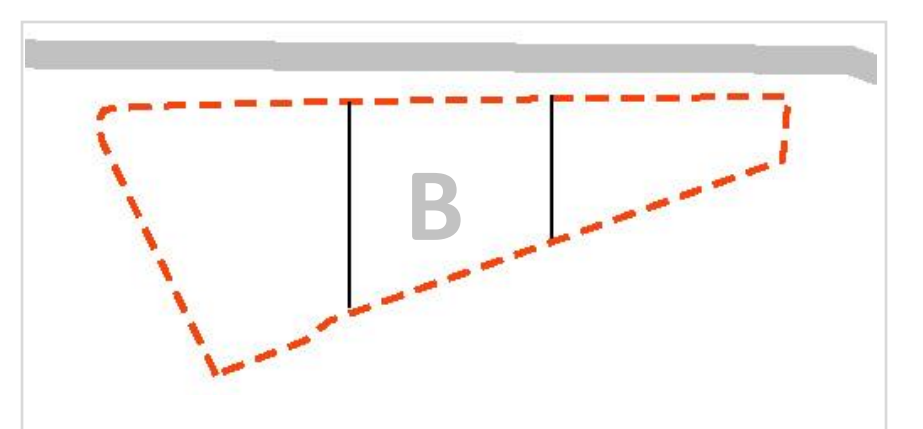
- Include placemaking public art in corner plaza
- Visually screen BART vents and equipment with landscape or art

Issues/ Challenges

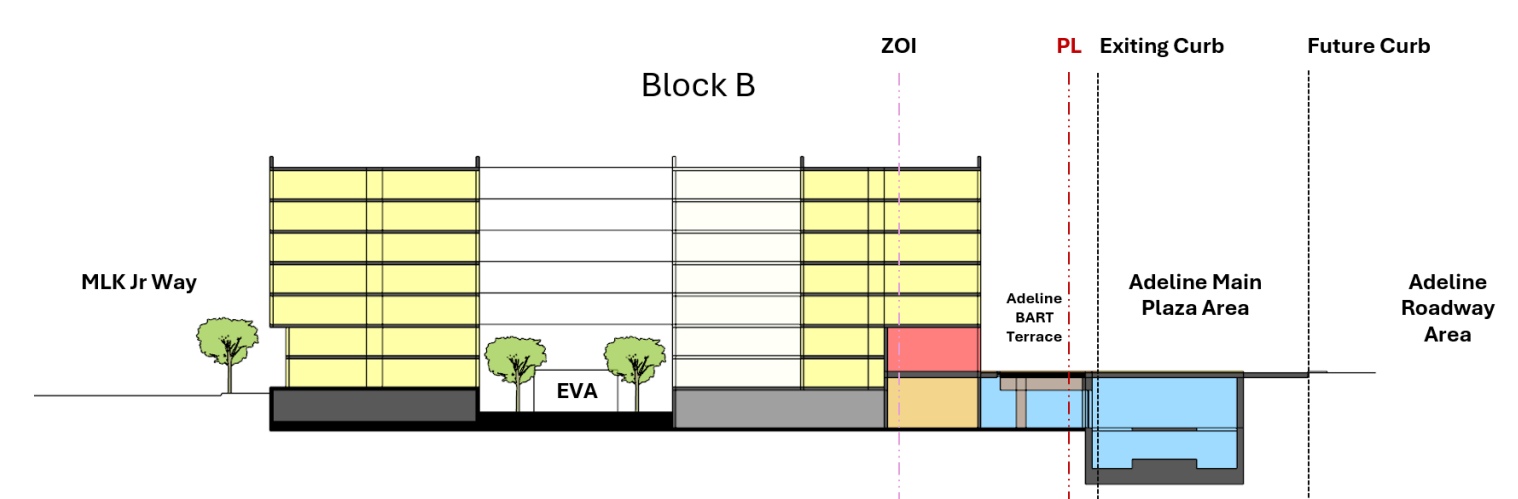
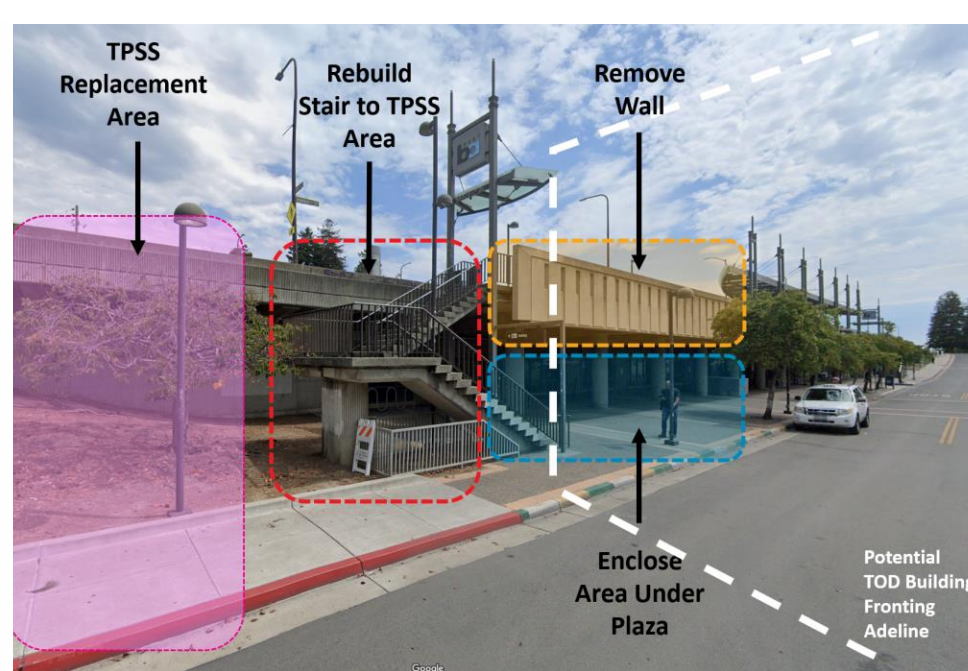
- TOD/Corner Plaza relocates ADA ramp (new ramp at Station Entrance Plaza)
- TOD/Corner Plaza removes existing trees
- Maintenance access needed to BART equipment and vents

Area B: Existing BART Terrace

Area B includes frontage between the TPSS Replacement Area and the Main Station Entrance. **Sample design concept only. No design for this project exists at this time.**



- +110' Estimated Site Elevation
- Estimated BART Property Line
- ▼ BART Station Entrance (below BART Terrace)
- ① Commercial Frontage along BART Terrace
- ② BART Terrace Extension
- ③ Bike Parking (below BART Terrace)
- ④ Vehicle Parking Podium
- ⑤ TPSS Replacement Area
- ⑥ North Plaza Stair (to be rebuilt)
- ⑦ Emergency Vehicle Access to Station Entrance Plaza
- ⑧ Potential Bridges over Emergency Vehicle Access



Sample Street Section through Development Area B

Opportunities

- Maximize building frontage connecting to the Adeline Main Plaza and BART Terrace
- Create active ground floor engaging with the Adeline Street and Plaza
- Enclose area under Adeline BART Terrace for bike parking

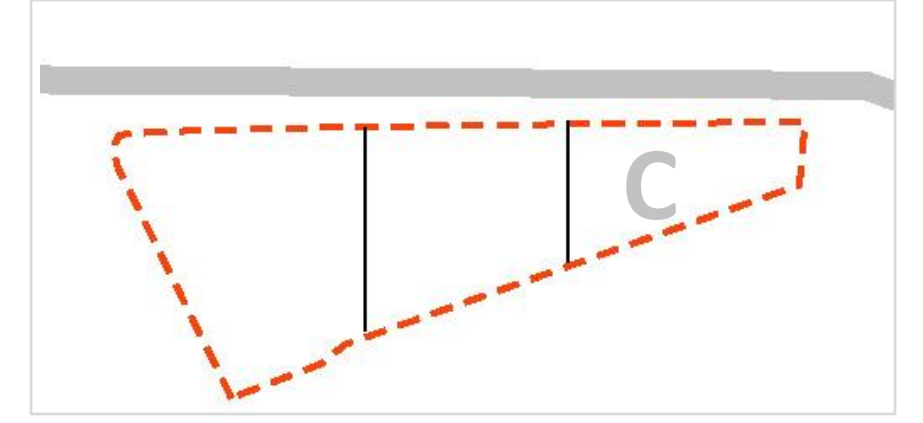
Issues/Challenges

- Special foundation may be required within station's Zone of Influence (ZOI)
- Unknown condition of existing concrete may complicate wall/railing removal for bridge connection

Activating Adeline Street

Area C: South of Station Entrance

Area C is south of the Main Station Entrance and south plaza stair where the existing bike station is located. Area C also includes access to a BART storage area and emergency exit stair to the station platform. The sidewalk along this area is cantilevered over the BART parking lot. **Sample design concept only. No design for this project exists at this time.**



- +110' Estimated Site Elevation
- Estimated BART Property Line
- ▼ South Platform Egress Stair Point
- ① Residential Development C Entrance
- ② South Plaza Extension
- ③ BART Vents
- ④ BART Intake Vent (to be extended 10' above plaza grade)
- ⑤ South Platform Egress Stair + BART Storage (access to be reconfigured)
- ⑥ Potential Emergency Egress and Storage Access Tunnel
- ⑦ Ashby BART Station Gateway Element
- ⑧ Ground Floor Commercial Frontage
- ⑨ Pedestrian/Bike Access Routes (<5% grade)

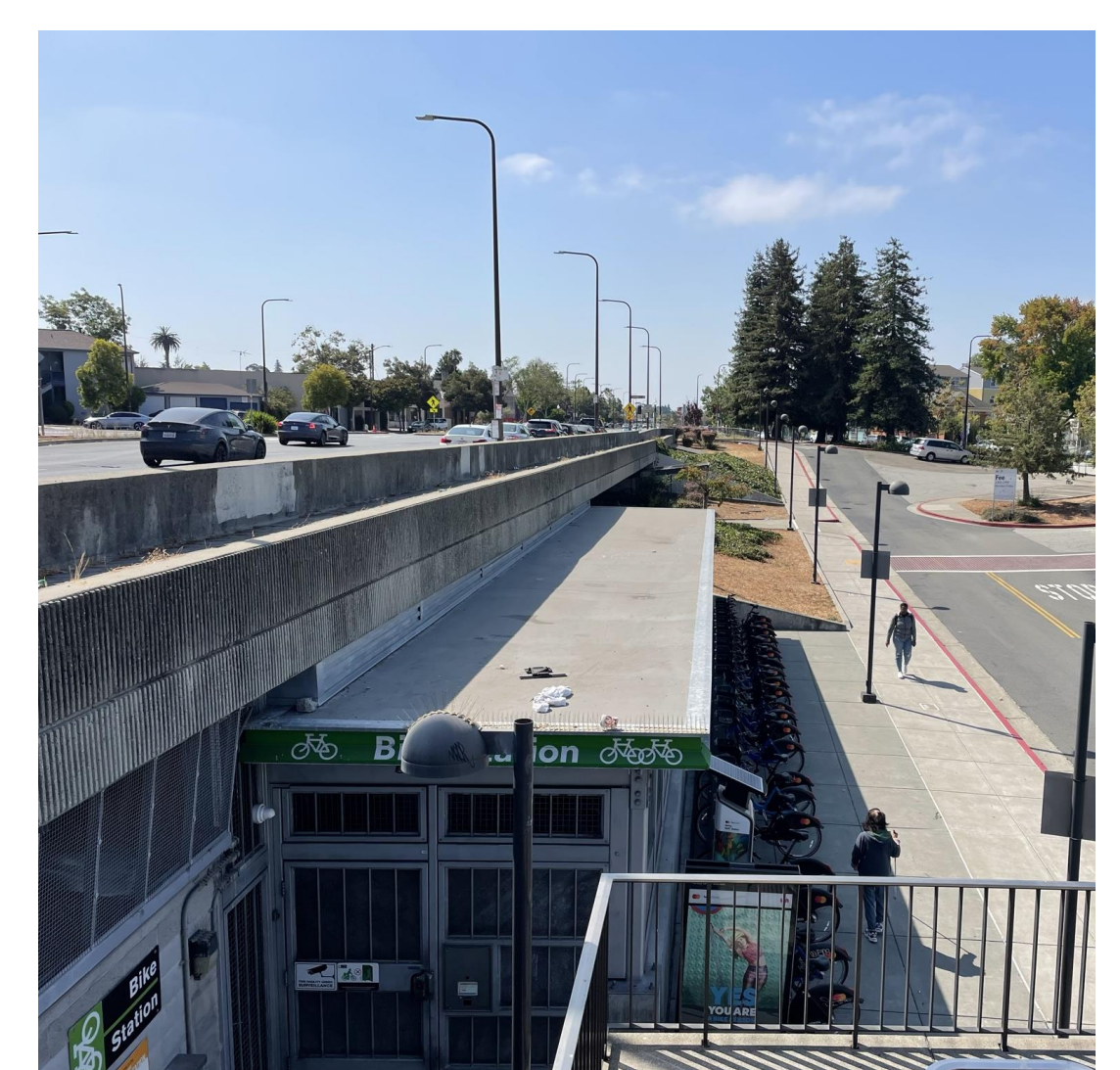
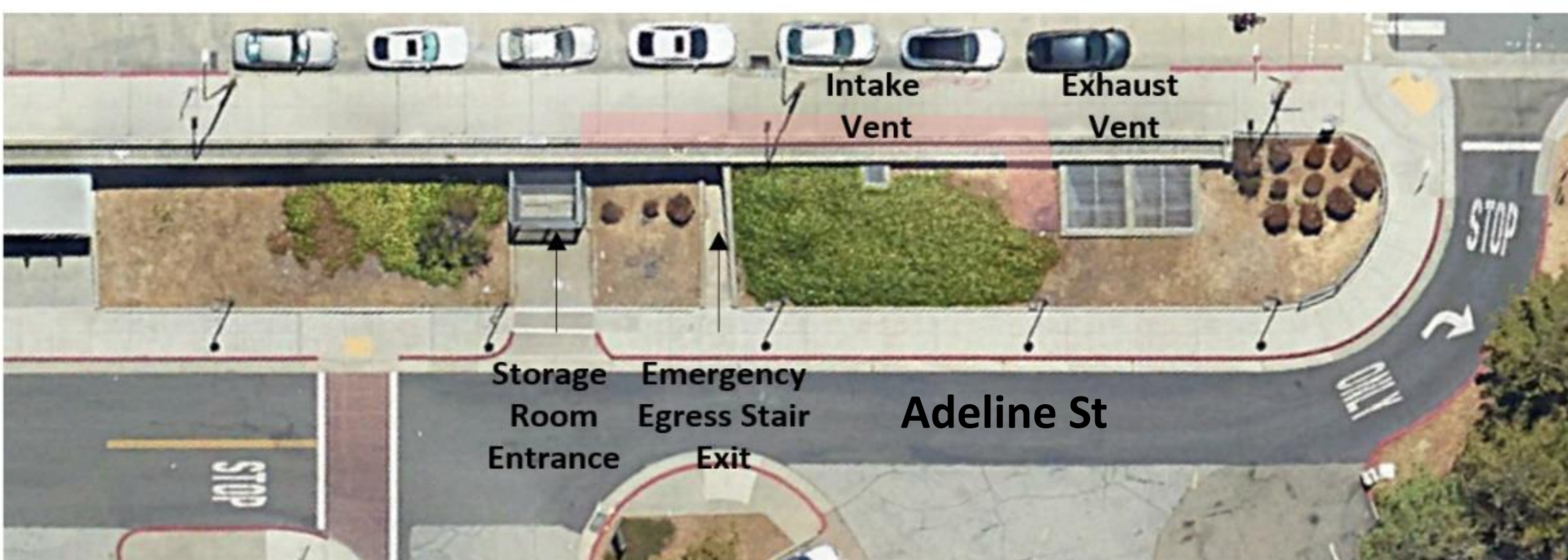
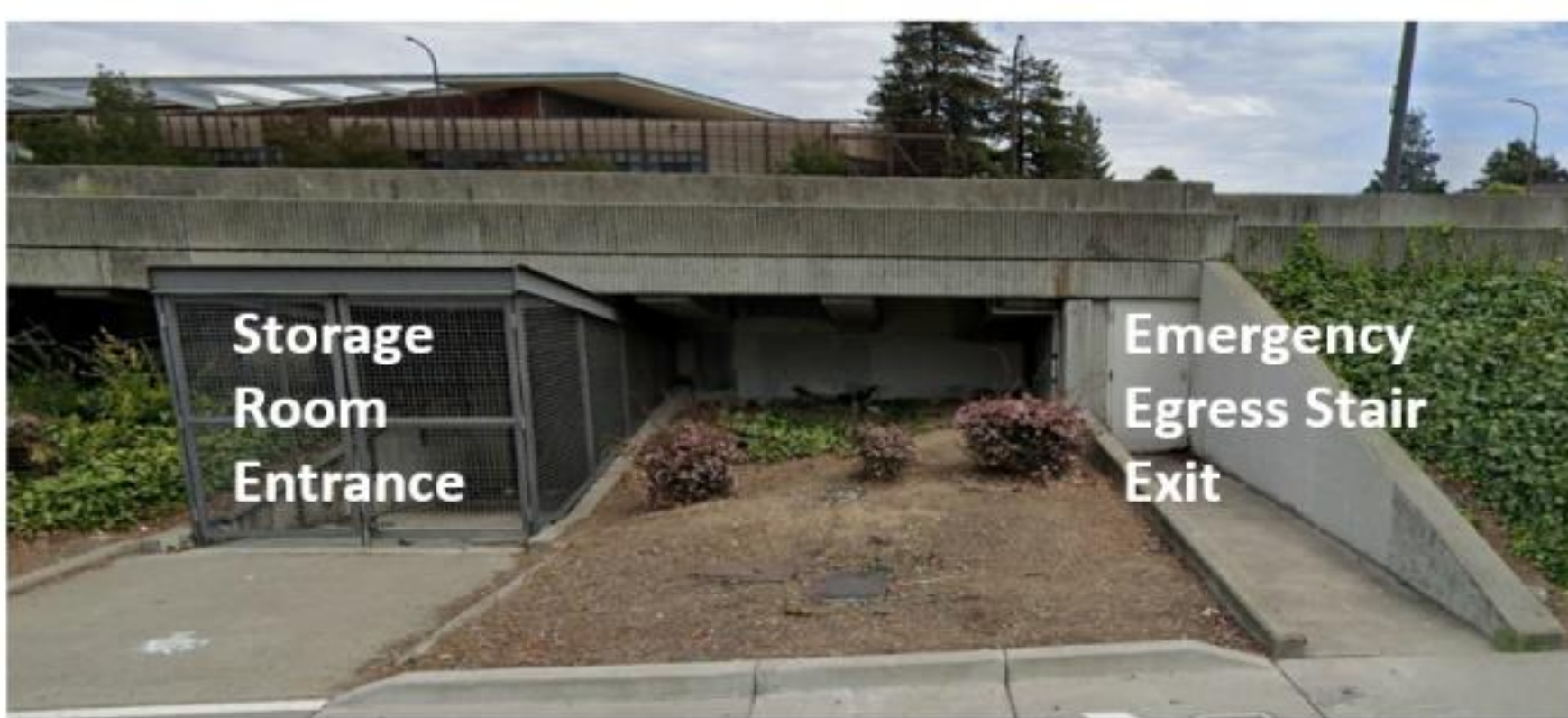
Opportunities

- The Adeline Main Plaza (created by the redesign of Adeline Street) may be extended to new building(s) by a new structure that connects the new development to the existing sidewalk that is cantilevered over the station entrance.
- The design of the Adeline Plaza Extension, Block C, and the Station Entrance Plaza's pedestrian and bike connection can and should be integrated to create a cohesive experience.

Issues/Challenges

- Special foundation may be required for buildings within station's Zone of Influence (ZOI).
- Unknown condition of existing concrete may complicate wall/railing removal for bridge connection
- BART vents will need to be modified and rebuilt to extend to the Adeline Plaza Extension.
- The exhaust vent redesign shall meet all venting requirements and shall vent a minimum of 10 feet above plaza level.
- Access to the South Platform Egress Stair and BART Storage Room shall be reconfigured and meet all access and fire/life safety requirements.

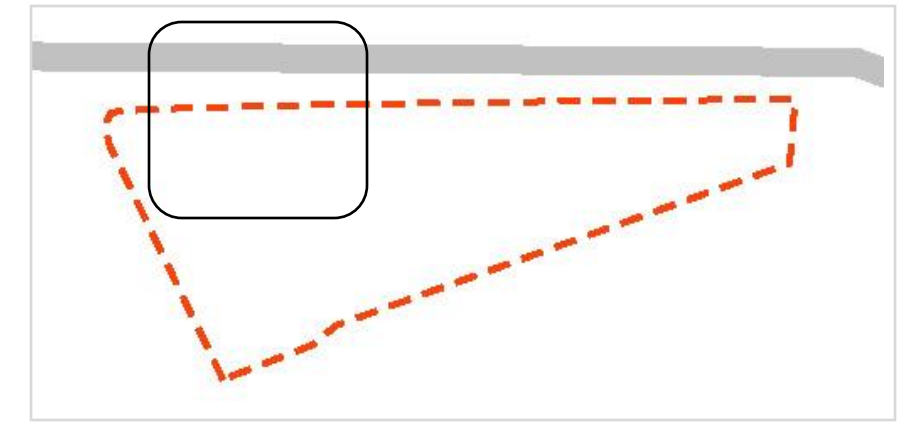
Existing Condition



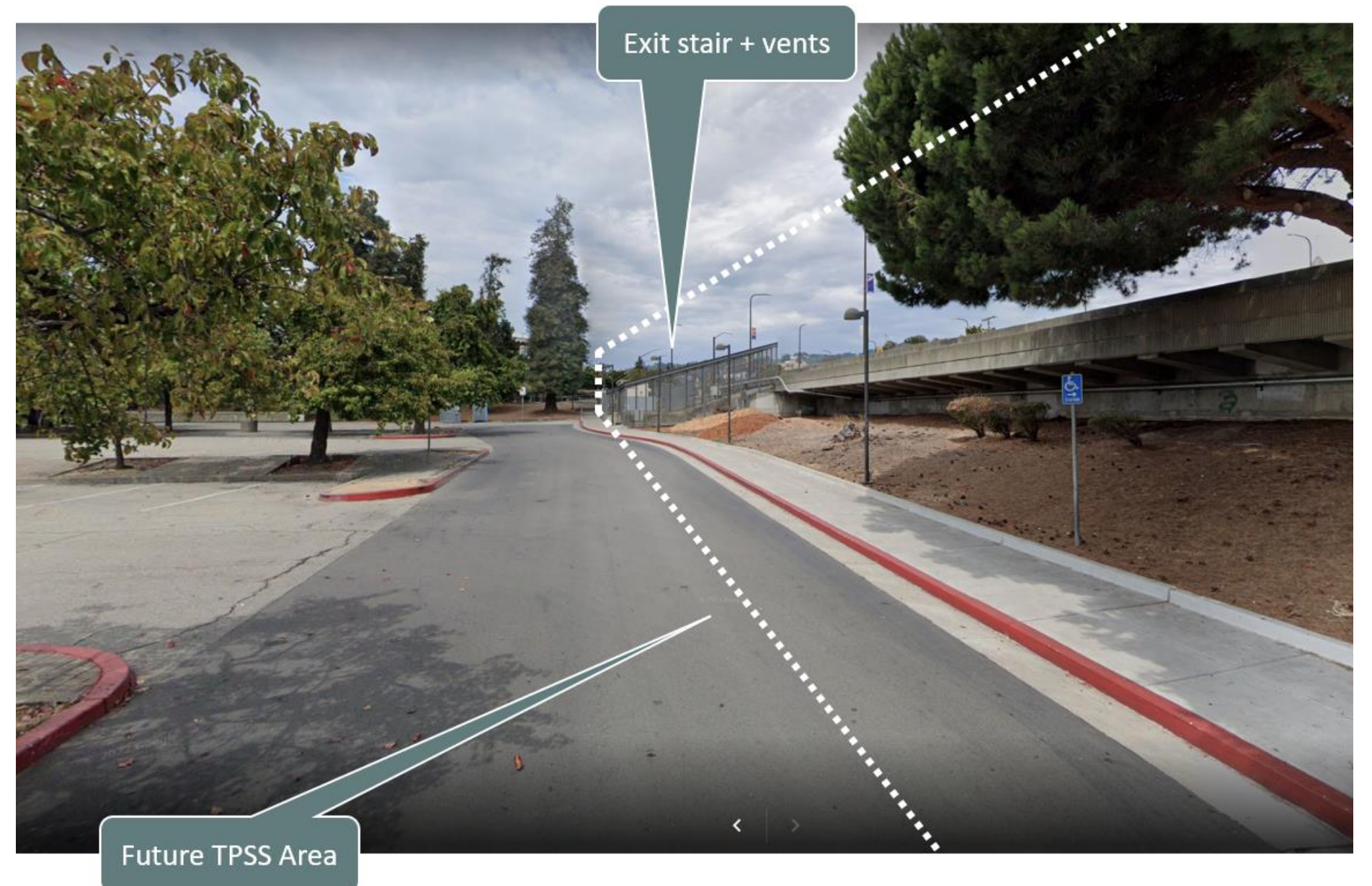
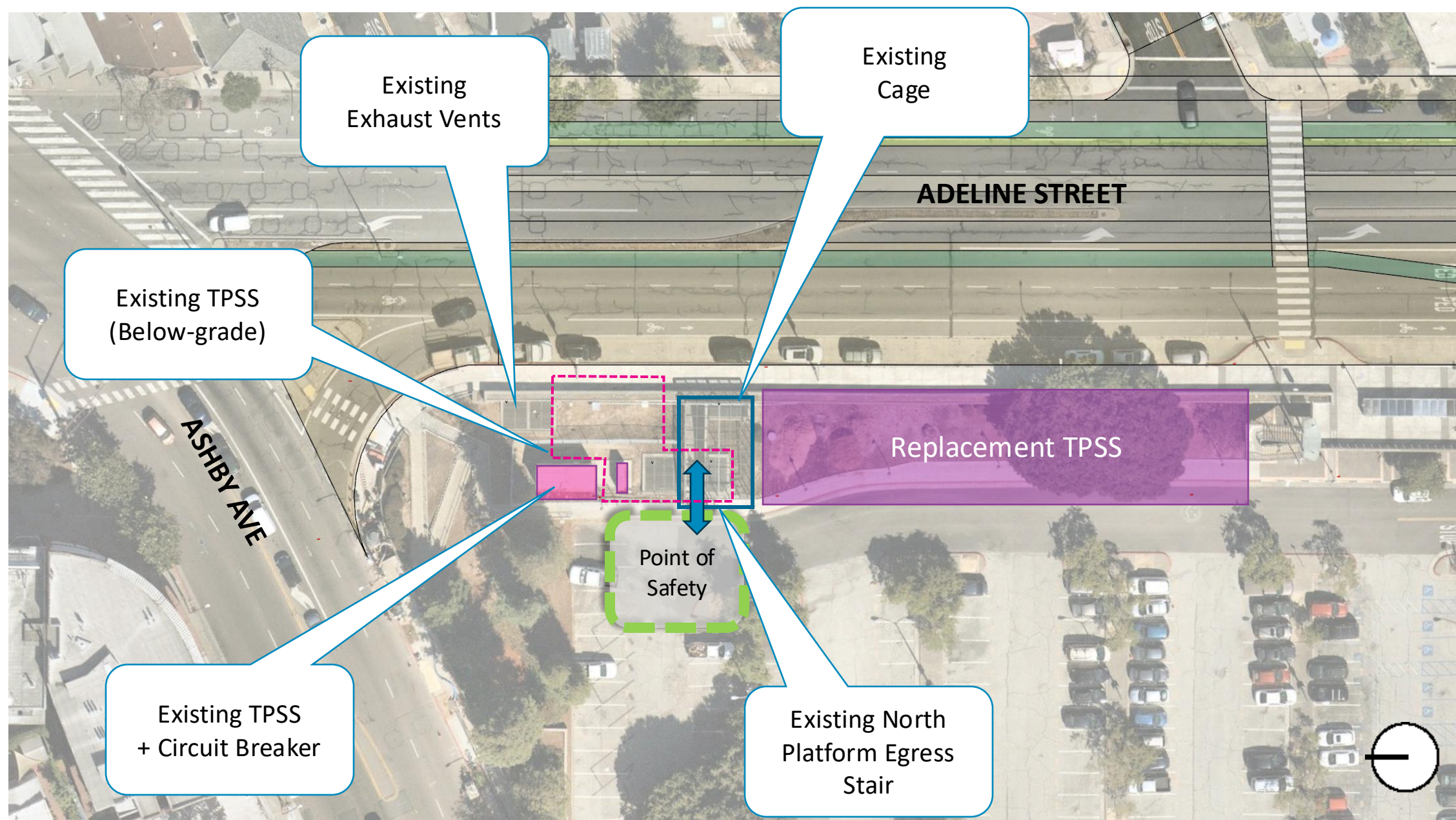
BART Traction Power Substation

Overview

Ashby Station is the location of one of BART's Traction Power Substations (TPSS) that provide the stable and constant power required to operate trains. But Ashby BART's underground substation is over 50 years old and must be replaced with a larger modern facility. The new Ashby BART TPSS has been fully designed and will be above ground along Adeline Street in the West Lot.



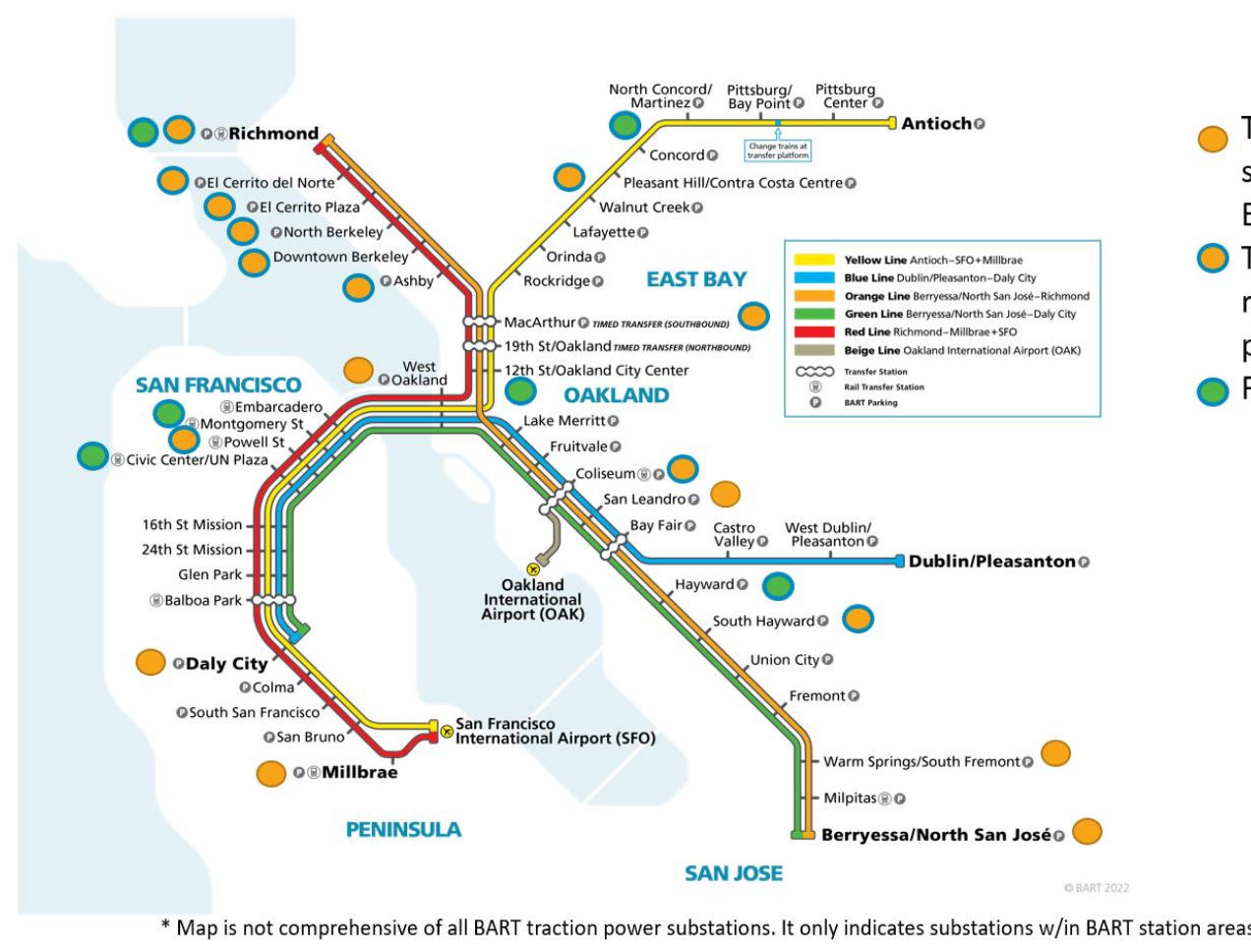
Ashby's TPSS: Now & future



Future TPSS Location along Adeline Street

Not just Ashby...BART is replacing many TPSS facilities

BART is replacing and upgrading traction power substations not only at Ashby Station, but throughout the system. These upgrades will allow BART to serve more passengers safely and reliably.



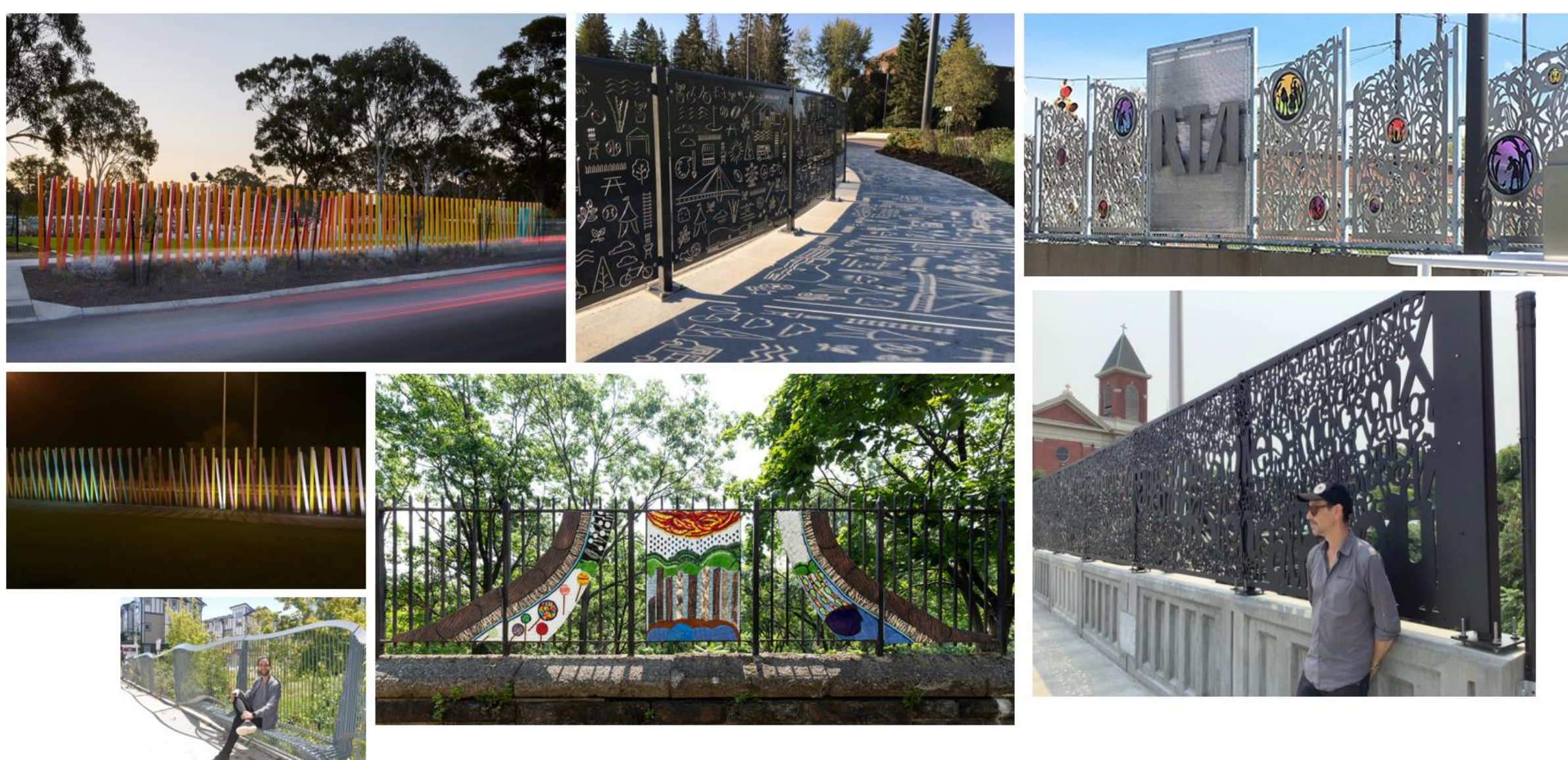
BART's Planned TPSS Upgrades

- Traction Power substations (TPSS) at BART stations
- TPSS w/upgrade & replacement projects
- Future New TPSS

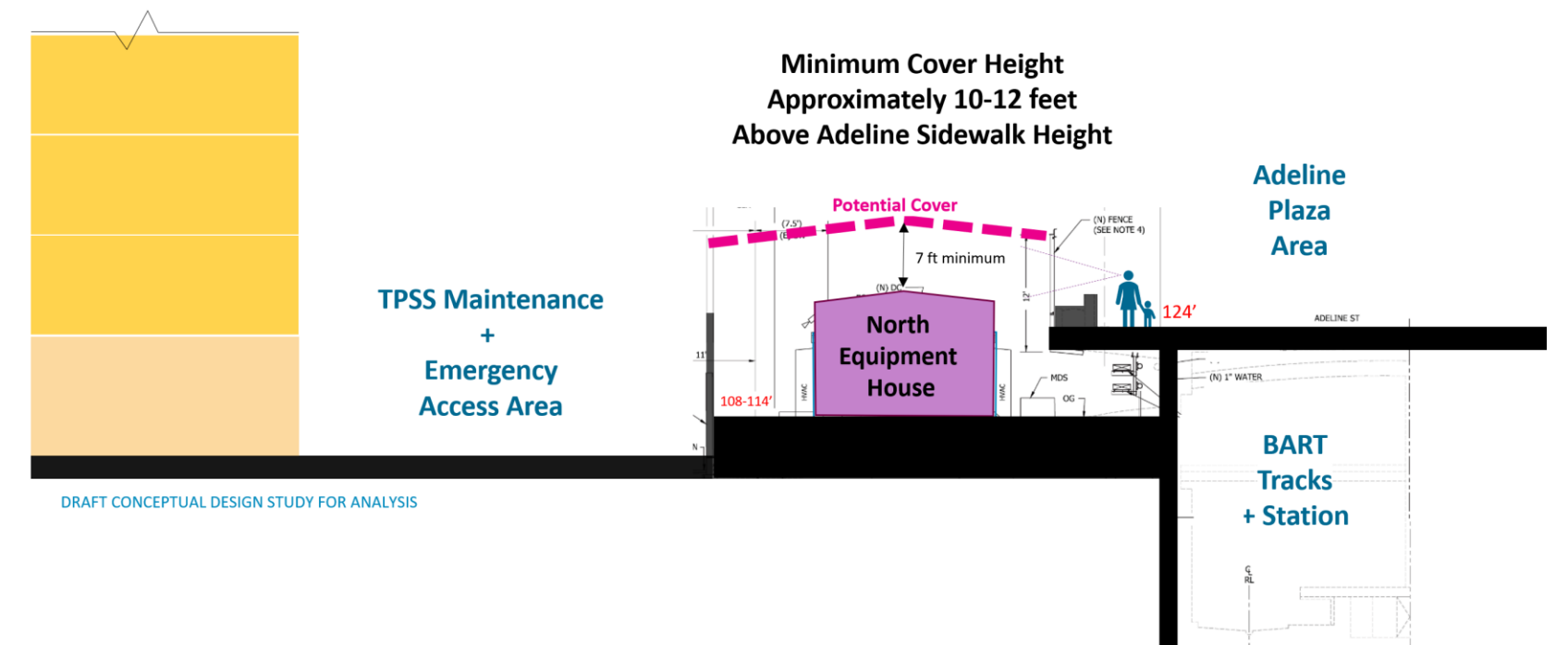


Example TPSS (Fremont Station)

Screening Ashby BART's TPSS from view



Examples of Potential Artistic Fence along Adeline Street



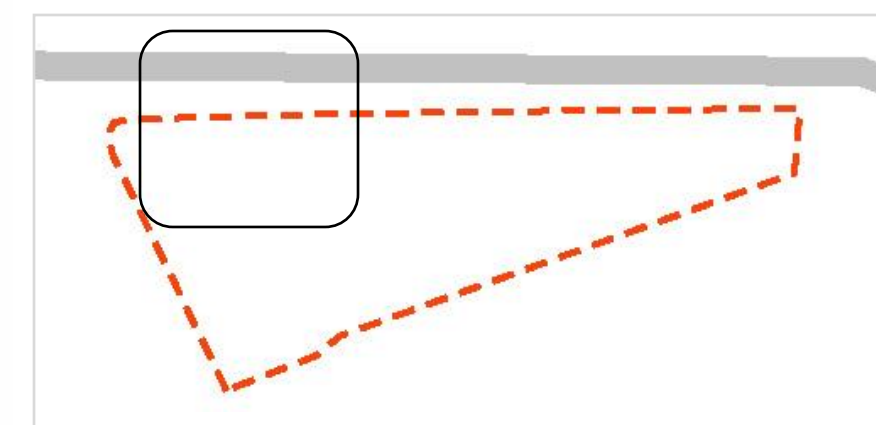
A cover over the TPSS was analyzed to see if the TPSS could be hidden from view at or below sidewalk grade. The cover was determined to be infeasible and impractical due to the required clearance height above equipment housing, which would raise the cover 10-12 feet above sidewalk grade – making it more visible instead of less.

BART Traction Power Substation

How will the TPSS fit into the new development?

Adjacent to the planned TPSS, BART requires areas for staging, crane access to service the TPSS, first responder access, and a temporary substation should one ever be needed. The size of these areas has been reduced in response to feedback from the public. The TOD buildings can wrap around these areas on three sides.

TPSS requirements & integration



- Estimated BART Property Line
- ▽ BART Emergency Egress Stair + Access
- ↔ TPSS Maintenance Access
- ↔ TPSS Equipment Access
- ① TPSS Equipment + BART Vents
- ② Access Pathway
- ③ TPSS Staging Area
- ④ TPSS Crane Area
- ⑤ Temporary TPSS Area
- ⑥ Emergency Vehicle Access
- ⑦ Emergency Vehicle Access/Maintenance Turn-around
- ⑧ North Plaza Stair (to be rebuilt)

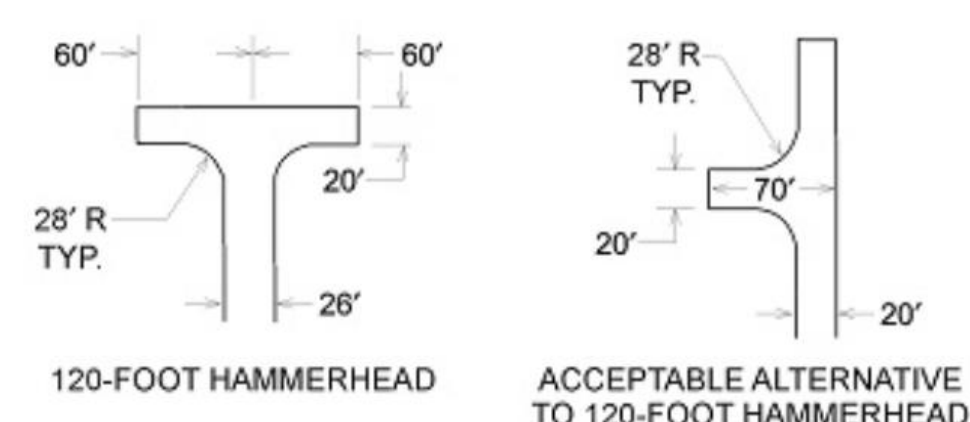
Sample design concept only. No design for Ashby BART exists at this time.

What is driving the design of the TPSS Staging Area?

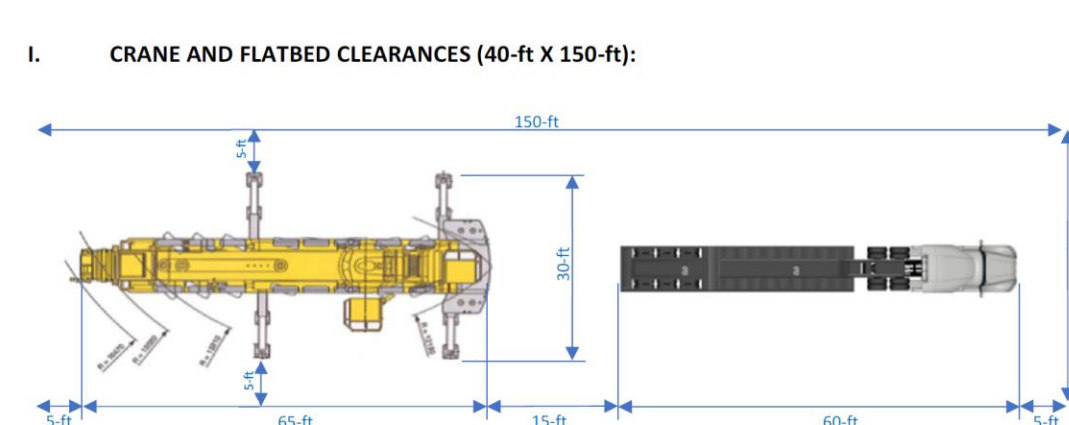
The TPSS requires space for several key functional elements shown below. In addition to the base size of these elements, equipment and vehicles need to get in and out. The largest area is for the crane and equipment loading. The team considered whether this area could be on the Adeline Main Plaza to minimize the impact on new West Lot buildings. Analysis by an independent structural engineer found that the crane staging area cannot be located on the future Adeline Street Main Plaza because of the structural impact on the BART station below. The sample design above represents the smallest area possible.

Key Functional Requirements:

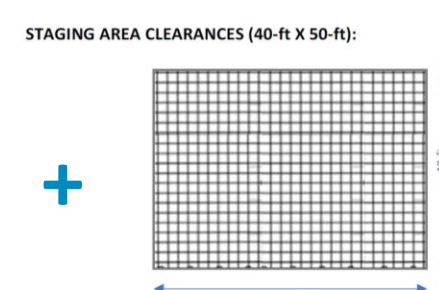
Emergency Vehicle Access & Turnaround



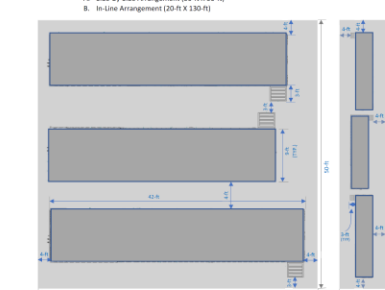
Crane + Equipment Loading Area



Maintenance Access & Staging Area



Temporary TPSS Area



TPSS Staging Area

190' x 50'